

# Future and next steps

We are your partners, let's collaborate



Green Rider Nosiso Faith Muni on a class 2 cycling lane in the CBD. Photograph by Julian Oldenberg, 2023.

Look at the back for more details on current and future Safe Passages



# Transportation Crisis and Solutions

## The Bigger Safe Passage Family

Walking is a primary mode of transport in the Global South, especially in Cape Town. However, it's hindered by inadequate infrastructure and safety issues. In Sub-Saharan African cities, a third of road deaths are pedestrians, a figure that rises to 57% in Cape Town. Additionally, despite cyclists being just 1% of Cape Town's road users, they account for 3% of road fatalities.

South Africa faces a transportation crisis, with South Africans spending an average of 4.5 hours commuting daily and transport costs taking up 40% of their income.

The Safe Passage Programme is more than a South African first public and private partnership, it is a programme of working with a range of stakeholders to bring both short and long-term solutions between our many informal and formal areas.

In the transportation sector, we are working with the following stakeholders to make mobility more accessible, safer and connected:



Listen to Roland Postma and Craig Atkinson (CEO of Green Riders) speaking to Clarence Ford about the Safe Passage Programme and how it will enable more green, youth jobs.



# What is the Safe Passage Programme?

The Safe Passage Programme, a partnership between the Suppliers Development Initiative (SDI) Micro-Enterprise Trust and Young Urbanists NPC, aims to address South Africa's transportation challenges and support the informal sector. It focuses on creating safer, affordable routes connecting informal and formal areas, aiming to counteract the Apartheid-era spatial planning and contemporary decisions that prioritize private cars and urban sprawl.

The Safe Passage includes but is not limited to:

- Safe and dedicated cycling lanes
- New intersections for pedestrians and cyclists
- NMT (Non-Motorized Transport) walkways
- Enhanced lighting
- Tactical Urbanism
- Traffic calming measures
- Universal Basic Urban Design principles
- Support for informal trading
- Enhancements for informal public transportation, especially taxis
- Integration of low-carbon vehicles for e-hailing, taxis, and other purposes
- Improvements in scholar transportation, including taxis, cycling, and walking
- Biodiversity and Water Sensitive Urban Design approach
- Community safety initiatives and programmes
- Maintenance of all the above infrastructures
- And much more.

# Safe Passage Partners



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Team: Young Urbanists, SDI Micro-Enterprise Trust

Transport Partners: Green Riders, Bicycle Mayor of Cape Town, Active Mobility Forum, Loop Taxi, PRSASA, Pedal Power Association, Bicycle Mayor of Cape Town, Fixie Fit SA, Dragons Sport, Rook and Langa Bicycle Hub.

Government Partners: City of Cape Town and City of Tshwane

Built Form Sector Partners: CityCon Africa and EFG Engineers

Other partners: Cedar Coffee and Urban Lime

# Safe Passage 02

## Masiphumelele to Fish Hoek

Located 40 km south of Cape Town, Masiphumelele is a beacon of determination on the Cape Peninsula. Its Xhosa name, meaning "let us succeed," embodies its resilient spirit.



Before, Kommelje Rd. Illustration by Axel Maas, 2023.

### Challenges:

Prevalent poverty, unemployment, and crime. Inadequate housing and limited safe mobility infrastructure for taxis, public transit, and Non-Motorised Transportation (NMT).

According to Irlam's 2016 study, taxis and walking dominate local commutes. Bicycles, especially for labourers, are cost-effective and reduce traffic. Yet, despite walking being a common mode (20-34% in Cape Town), challenges like insufficient infrastructure and safety concerns remain.

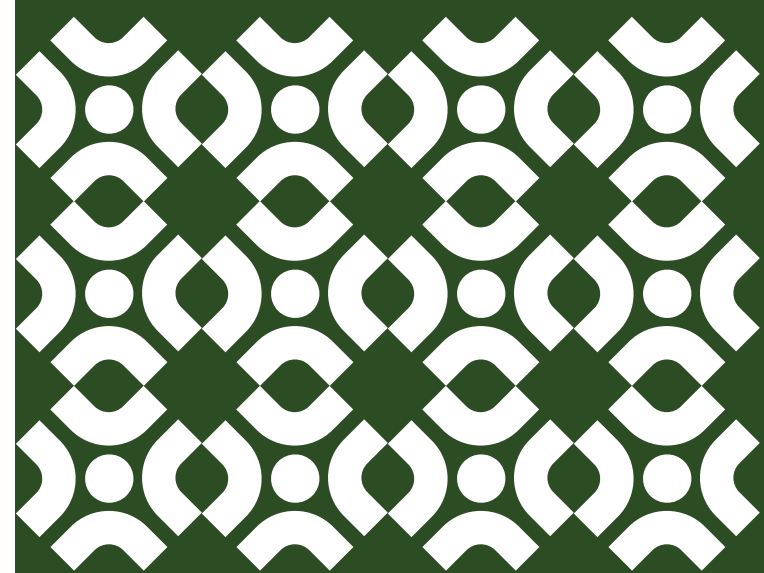
The Masiphumelele Safe Walkway 01 project addresses these challenges by **proposing wider pathways, safer intersections, tactical cycling lanes and improved lighting.**

Irlam, J. 2016. Barriers to cycling mobility in Masiphumelele, Cape Town: a best-worst scaling approach. University of Cape Town.



# Safe Passage Programme

Empowering  
Micro-Economies,  
One Journey at a Time.





# Current Safe Passage Programme's

## From the Cape to our Capital City

### Safe Passage 01



Langa to CBD Map by Axel Moos, 2023.

**Langa to the Central Business District (CBD), Cape Town**

**Status:** In process  
**Support:** Executive Mayor Cape Town

#### Phase 01

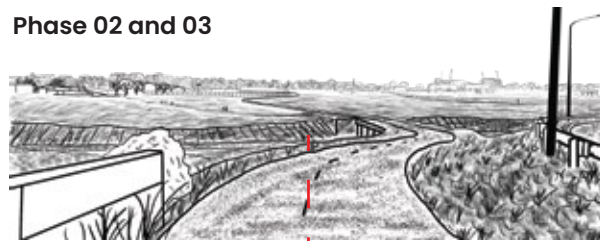
The Safe Passage Programme has secured approval for over 600 tactical bollards to enhance safety between Liesbeek and Woodstock train stations for the Albert Road cycling lane.

Approved by Urban Mobility and set for construction later this year, the project supports the SDI Micro Trust beneficiaries Green Riders, ensuring e-bike delivery riders have safe access to the CBD and nearby areas. **The red dotted line shows where they will go.**



Once the bollards are in place, or after any subsequent phase is completed, the Safe Passage Programme will take responsibility for maintaining the infrastructure. Additionally, we plan to offer support programmes, such as providing security for scholars and pedestrians between Langa and Athlone, as well as parts of the Albert Cycling lane and the Melkbos Cycling Highway that connect with Safe Passage 01.

### Phase 02 and 03



Before, Langa to Athlone. Illustration by Axel Moos, 2023.



After, Langa to Athlone. Illustration by Axel Moos, 2023.

Phase 02 and 03 will complete the Safe Passage between Langa and the CBD, benefitting cyclists, pedestrians, scholars, and taxis. We're in the concept phase, looking to complement projects like the Salt River station NMT upgrades and the Liveable Urban Waterways Programme (LUWP). Collaborating with different city of Cape Town directors, sub-councils, communities, and stakeholders, we aim for feasible upgrades and community buy-in. **We're considering low-cost solutions, like a soccer field between Langa and Athlone, as indicated on the map.**

### Safe Passage 02



Masi to Fish Hoek Map by Axel Moos, 2023.

**Masiphumelele to Fish Hoek, Cape Town**

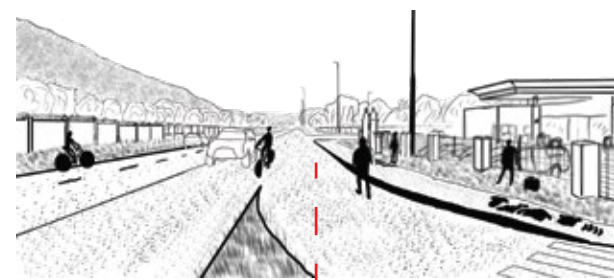
**Status:** In process  
**Support:** Sub-Council 19

#### Phase 01

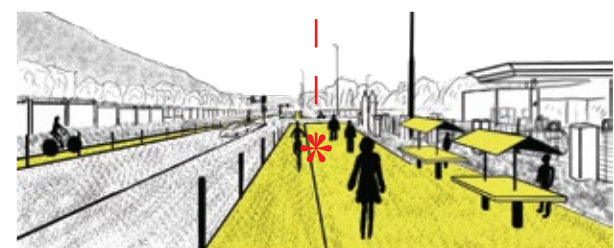
We're setting up two safe NMT routes for pedestrians, cyclists, traders, and taxi users. Given that most Masiphumelele residents use taxis, with 44% walking and 16% cycling, redesigning Kommetjie Road for pedestrian safety is crucial.

#### Phase 02

This phase will improve the class 3 cycling lane, redesign intersections, and transition taxis to greener vehicles, partnering with the taxi industry for better regulations and support for both commuters and scholars.



Before, Kommetjie Rd. Illustration by Axel Moos, 2023.



After, Kommetjie Rd. Illustration by Axel Moos, 2023.

### Safe Passage 03



Mamelodi to Pretoria East Map by Axel Moos, 2023.

**Mamelodi to the Pretoria East, Pretoria**

**Support:** City of Tshwane  
**Status:** In process

#### Phase 01

We've partnered with the City of Tshwane and our local partners, City ConAfrica to develop an NMT route from Mamelodi to Pretoria East. **A key goal of Phase 01 is ensuring the Lynwood and Solomon Mahlangu Drive intersection is free from load-shedding.** Construction on this intersection begins very soon, with plans for a safer NMT corridor focused on pedestrians, public transport, and cyclists next year.



#### Phase 02

Phase 02 aims to expand the programme, making more intersections load-shedding-free due to the energy crisis. We'll also refocus intersections on NMT and public transport, including taxis. Separately, after meeting with Tshwane's Mayor, Cilliers Brink, we're considering a CBD renewal plan inspired by street experiments.

# Future Safe Passage's and Next Steps

### Safe Passage 04, 05, etc.

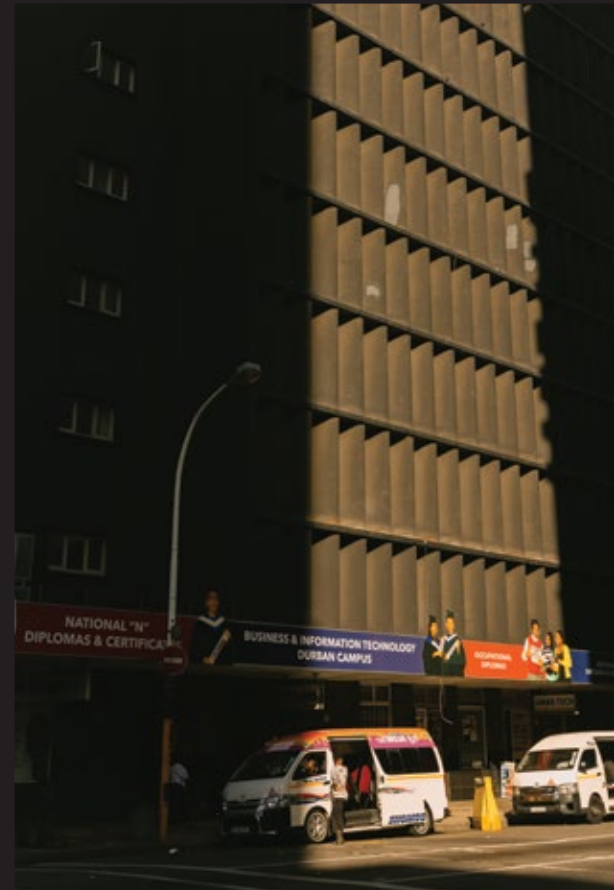


Tactical Urbanism for Safer Streets, New York 1981.

#### The Future:

We're just beginning, but we aim to expand this public-private partnership nationwide. We're devoted to fulfilling the National Development Plan, collaborating with all government tiers to address stark inequality and foster job creation via sustainable Safe Passages. We're also part of the Just Energy Transition Investment Plan (JET IP) and seek to grow the programme using corporate sponsorships, leveraging ESG funds, branding rights, outdoor advertising, and heritage urbanism. Which potential Safe Passage would you like to nominate?

- CBD Renewal, City of Tshwane
- Alexandra to Sandton, City of Johannesburg
- Where do you want Safe Passage next?



Durban Taxi. Photograph by Dan Carter, 2023.

## How To Get Involved And Nominate A Safe Passage?

We're here to listen as the Safe Passage Collective which consists of a team of seasoned professionals. Contact us anytime to arrange a meeting to see what can be done. We aim to collaborate, do more, and talk less.

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